

Madison, Wisconsin

CITY OF MADISON

CITY ENGINEERING DIVISION

DEPARTMENT OF PUBLIC WORKS

PLAN OF PROPOSED IMPROVEMENT

INDEX OF SHEETS

SHEET NO.	TITLE
1	TITLE
D1	STANDARD NOTES
D2	TYPICAL SECTIONS & DETAILS
P1	SITE LAYOUT PLAN
CL3	GRADING PLAN
E1	ELECTRICAL PLAN
L1	LANDSCAPING PLAN

2505 UNIVERSITY AVENUE

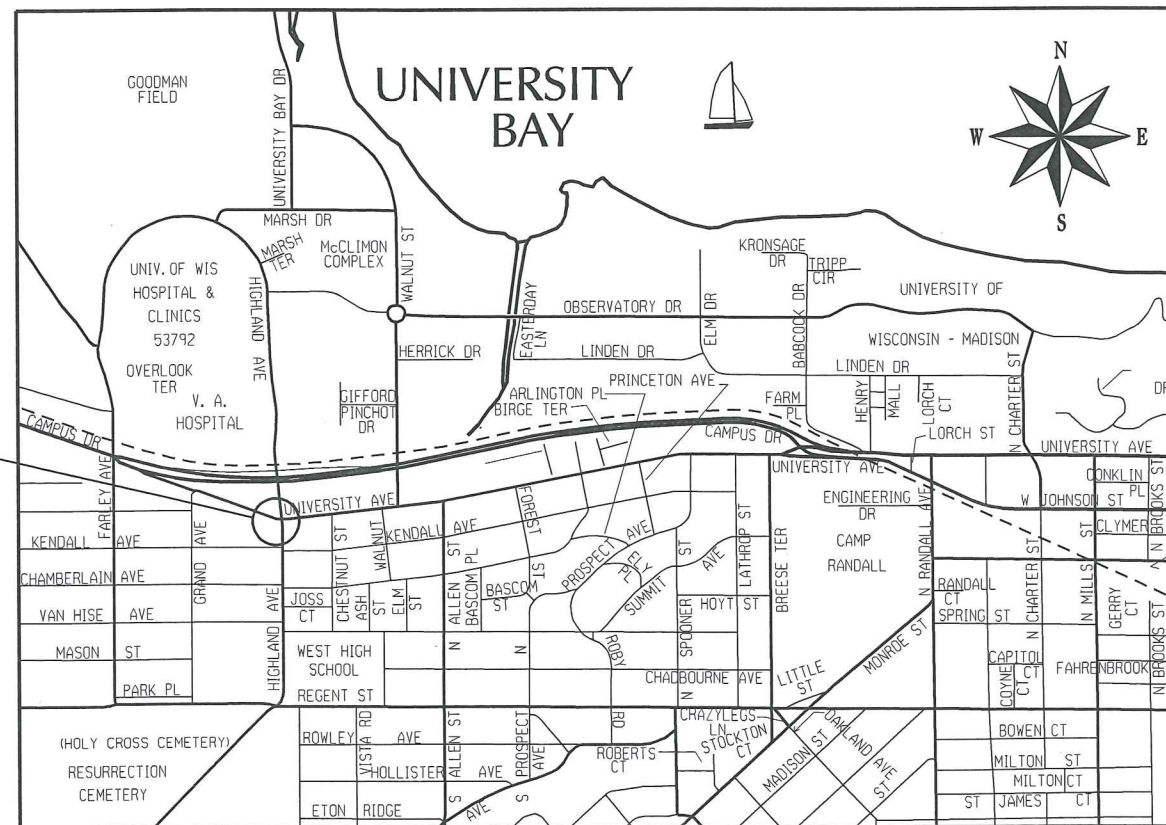
CITY PROJECT NO. 53B2425

CONTRACT NO. 2425

MUNIS NO. 10834

CONVENTIONAL SIGNS	
FIELD VERIFY ALL UTILITY LOCATIONS	
GAS	— G —
STORM SEWER	— ST —
SANITARY SEWER	— SAN —
WATER	— W —
OVERHEAD ELECTRIC	— OH —
POWER POLE	□
ADA COMPLIANT RAMP W/ DETECTABLE WARNING FIELD	■ ■ ■ ■ ■
COMBUSTIBLE FLUIDS	☀

PROJECT
LOCATION



NOTES:

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, CURRENT EDITION.

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADE OF 0.50% TOWARD STORM SEWER INLETS.

SIDEWALK RAMPS AND CURB THRU SIDEWALK RAMPS SHALL HAVE A MAXIMUM SLOPE OF 1" PER 12". SIDEWALK AND CURB RAMPS SHALL BE CONSTRUCTED WITH A SIDE SLOPE OF 1.50%. SIDEWALK SHALL HAVE A MINIMUM LONGITUDINAL SLOPE OF 0.50% AND A MAXIMUM LONGITUDINAL SLOPE OF 5.00% EXCEPT WHERE STREET GRADES EXCEED 5.00%.

PUBLIC IMPROVEMENT PROJECT APPROVED

JULY 21, 2015

BY THE COMMON COUNCIL
OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN
APPROVED BY:

Chris Petrus for 10/21/15
City Engineer Report Date

RIGHT-OF-WAY IMPROVEMENT PLANS
DESIGNED BY:



ELECTRICAL PLANS
DESIGNED BY:



DETAILS

STANDARD NOTES
AND COMMENTS

CITY OF MADISON

ALL ASPHALT PAVEMENT ON UNIVERSITY AVE AND HIGHLAND AVE SHALL BE TYPE "C" ASPHALT PAVEMENT PER S.D.D. 4.06.

ALL ISLANDS SHALL HAVE TYPE "H" CURB & GUTTER PER S.D.D. 3.08 UNLESS NOTED OTHERWISE IN THE PLAN. ALL ISLANDS SHALL HAVE MOUNTABLE NOSES PER S.D.D. 3.13. ALL OTHER CURB & GUTTER SHALL BE TYPE "A" PER S.D.D. 3.06.

ISLAND INTERIORS SHALL BE OF SOD WHERE THE BACK TO BACK OF CURB DIMENSION IS GREATER THAN 6'.

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADES OF 0.5% TOWARD STORM SEWER INLETS.

PAVEMENT CROSS SLOPES SHALL BE 2%, TERRACES SHALL SLOPE AT A 4% GRADE TOWARD THE GUTTER, SHOULDERS SHALL HAVE A 4% CROSS SLOPE AND CROSS SLOPES FORMING DITCHES SHALL NOT EXCEED 4.00:1 UNLESS OTHERWISE INDICATED.

THE CROSS SLOPE OF SIDEWALKS AND BARRIER FREE SIDEWALK CURB RAMPS SHALL BE 1.5%. THE LONGITUDINAL GRADE OF BARRIER FREE SIDEWALK CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.03. AT ALL OTHER LOCATIONS THE LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED 5.0 % OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER NOR BE LESS THAN 0.5% AND SHALL DRAIN TOWARD STORM SEWER INLETS. CONCRETE TERRACE SHALL BE INSTALLED BETWEEN THE SIDEWALK AND THE BACK OF CURB AT SIDEWALK LOW POINTS WHICH CANNOT OTHERWISE BE DRAINED DIRECTLY TO THE GUTTER. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 4.00:1. ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND SET TO COMPLY WITH THE CITY OF MADISON STANDARD SPECIFICATIONS AND THE A.D.A. GUIDELINES.

OBTAIN A PRINT OUT OF THE ALIGNMENT FROM THE CITY ENGINEER PRIOR TO STAKING THIS PROJECT.

CURB STATION AND OFFSETS SHALL BE TO THE FACE OF CURB UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE TOP OF CURB (OR EXTENDED TOP OF CURB FOR DRIVEWAYS OR RAMPS) UNLESS OTHERWISE INDICATED.

POWER POLES AND OTHER OBSTRUCTIONS SHALL BE MOVED TO PROVIDE 2 FEET MINIMUM OF CLEAR DISTANCE FROM ANY FACE OF CURB OR EDGE OF SIDEWALK.

ANY INFORMATION SHOWN ON THIS PLAN, WHICH IS NOT PART OF THIS PROJECT, IS PRELIMINARY AND NOT FOR CONSTRUCTION.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.

ALL PERMANENT SIGNING AND POSTING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL PROVIDE, INSTALL AND MAINTAIN ALL STREET END BARRICADES, SIGNING AND TRAFFIC CONTROL, AS REQUIRED BY THE CITY TRAFFIC ENGINEER.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

CURB ON CUL DE SACS SHALL BE INSTALLED ACCORDING TO SDD 3.05.

THE LOCATION AND INFORMATION FOR PROPOSED NEW TREES, IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS ARE APPROXIMATE AND ARE SHOWN FOR REFERENCE ONLY. THE LOCATIONS, SPECIFICATIONS AND PLANTING METHODS OF ALL PROPOSED NEW OR REPLACEMENT TREES IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE APPROVED BY THE CITY FORESTER PRIOR TO INSTALLATION.

NO TREES IN THE RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE TRIMMED, PRUNED, REMOVED OR ADVERSELY AFFECTED IN ANY WAY UNTIL THE DEVELOPER HAS RECEIVED WRITTEN PERMISSION FROM THE CITY ENGINEER OR CITY FORESTER. SAID WRITTEN PERMISSION SHALL INCLUDE LANGUAGE INDICATING THAT SECTION 10.101 OF THE MADISON GENERAL ORDINANCES AND ADMINISTRATIVE PROCEDURE MEMORANDUM NO. 6-2, REFERING TO NOTIFICATION OF PROPERTY OCCUPANTS AND/OR OWNERS, HAS BEEN COMPLIED WITH.

STREET TREES SHALL BE REPLACED AS SHOWN IN THE PLAN OR AS DIRECTED BY THE CITY FORESTER. COORDINATE STREET TREE PLANTING, LOCATIONS, QUANTITY, SPECIES AND SIZE WITH DEAN KAHL OF CITY FORESTRY, 266-4891.

THE DEVELOPER SHALL REPLACE ALL CURB & GUTTER AND SIDEWALK THAT IS DAMAGED OR DETERMINED TO BE IN UNSATISFACTORY CONDITION.

THESE PLANS ARE FOR WORK WITHIN THE PUBLIC RIGHT-OF-WAY ONLY. INFORMATION SHOWN ON PRIVATE LANDS IS FOR REFERENCE ONLY.

ALL PAVEMENT SHALL BE PATCHED IN ACCORDANCE WITH CITY PAVEMENT PATCHING CRITERIA AND SHALL BE ACCOMPLISHED AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. ALL SAWCUTS FOR PAVEMENT PATCHES SHALL BE LOCATED AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER.

ALL DISTURBED TERRACE AREAS SHALL BE RESTORED WITH 4-INCHES OF TOP SOIL AND SEED UNLESS DIRECTED OTHERWISE ON THE PLAN OR BY THE CITY CONSTRUCTION ENGINEER.

THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN FOR REVIEW TO THE CITY TRAFFIC ENGINEERING DIVISION AT LEAST 10 WORKING DAYS PRIOR TO WORK BEGINNING WITHIN THE UNIVERSITY AVE OR HIGHLAND AVE RIGHT OF WAY. THE TRAFFIC CONTROL PLAN SHALL COMPLY WITH ALL MUTCD REQUIREMENTS. WORK SHALL NOT PROCEED ON UNIVERSITY AVE OR HIGHLAND AVE UNTIL THE TRAFFIC CONTROL PLAN HAS BEEN APPROVED. THE TRAFFIC CONTROL PLAN MAY INCLUDE BUT IS NOT LIMITED TO: LANE CLOSURE RESTRICTIONS, PEAK HOUR WORKING RESTRICTIONS, ACCESS REQUIREMENTS, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PHASING WORK AS NECESSARY TO MEET THE REQUIREMENTS OF THE APPROVED TRAFFIC CONTROL PLAN. CONTACT TOM MOHR, 267-8725, WITH CITY TRAFFIC ENGINEERING TO COORDINATE DEVELOPMENT AND APPROVAL OF THE TRAFFIC CONTROL PLAN.

Pavement Patching Criteria

Criteria use on:

Streets with pavement rating > 6
Arterial Streets

1. Length of Patch

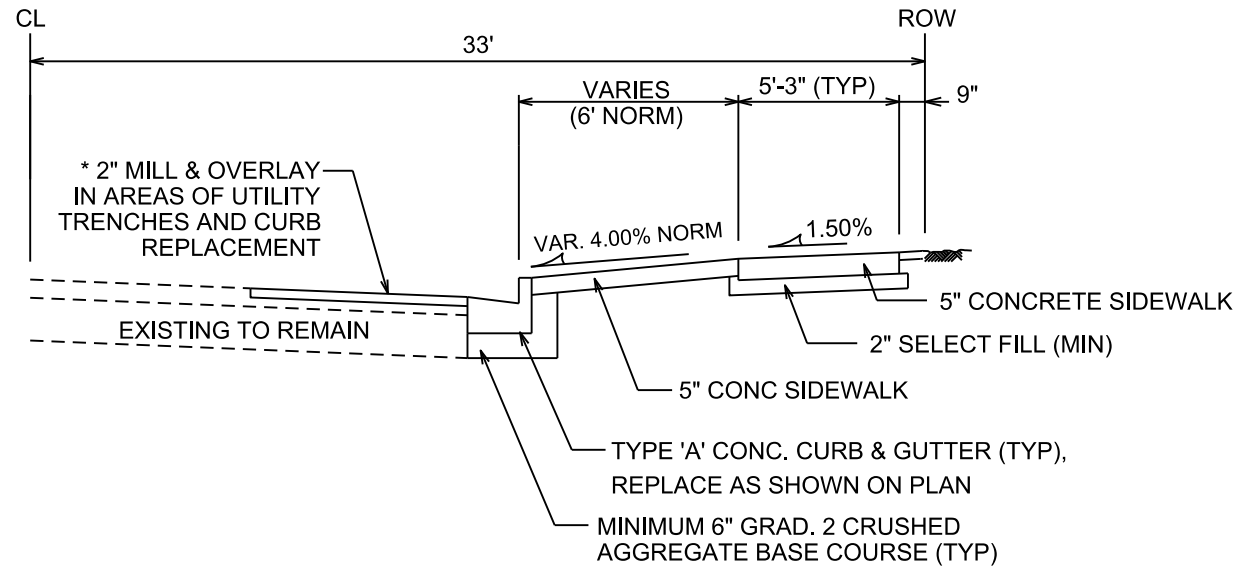
- a. Minimum 50 feet long
- b. Minimum of 15 feet beyond the excavation
- c. Where multiple patches are created and the separation between them is less than 100 feet, the patches shall be combined into a single patch.
- d. The patches shall be adjusted in the field to meet special conditions such as previous paving or patching limits.

2. Width of Patch (all dimensions are curb face to curb face)

- a. All Streets Except Divided or one way roadways
 - i. Street Width 0 to 24 feet wide - Patch entire street width
 - ii. Street Width 25 to 37 feet wide - Patch one half the street width (curb to centerline of roadway). Note - Utility Engineer may adjust paving limit to correspond with a painted centerline in situations where the painted centerline is not in the center of the street.
 - iii. Street Width 38 feet and up - Patch width of entire lane for each lane which was disturbed by the excavation.
 - 1. If the lane is adjacent to a bike lane, include the bike lane. (except when there is a parking lane between the bike lane and the curb)
 - 2. If the lane is a bike lane and adjacent to a parking lane, include the parking lane.
 - 3. If the lane is a bike lane and not adjacent to a parking lane, include the adjacent travel lane.
- b. Divided Roadways and One Way Streets
 - i. Street 0 to 19 feet wide - Patch entire street width
 - ii. Street Width 20 feet and up - Patch width of entire lane for each lane which was disturbed by the excavation.
 - 1. If the lane is adjacent to a bike lane, include the bike lane. (except when there is a parking lane between the bike lane and the curb)
 - 2. If the lane is a bike lane and adjacent to a parking lane, include the parking lane.
 - 3. If the lane is a bike lane and not adjacent to a parking lane, include the adjacent travel lane.

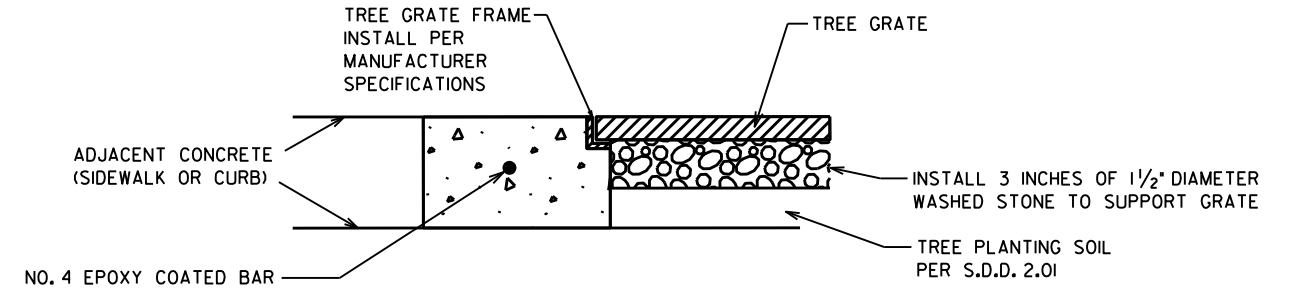
STREET RATINGS

STREET NAME	CLASSIFICATION	PAVEMENT RATING
UNIVERSITY AVE	ARTERIAL	7
HIGHLAND AVE	LOCAL	8

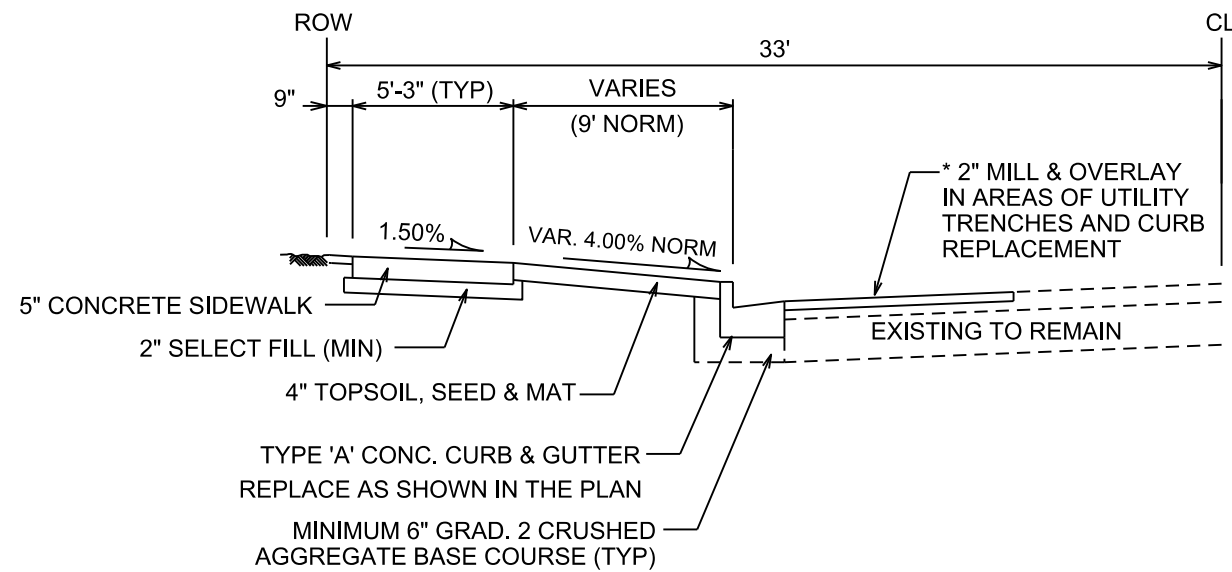


TYPICAL SECTION

UNIVERSITY AVE
NOT TO SCALE

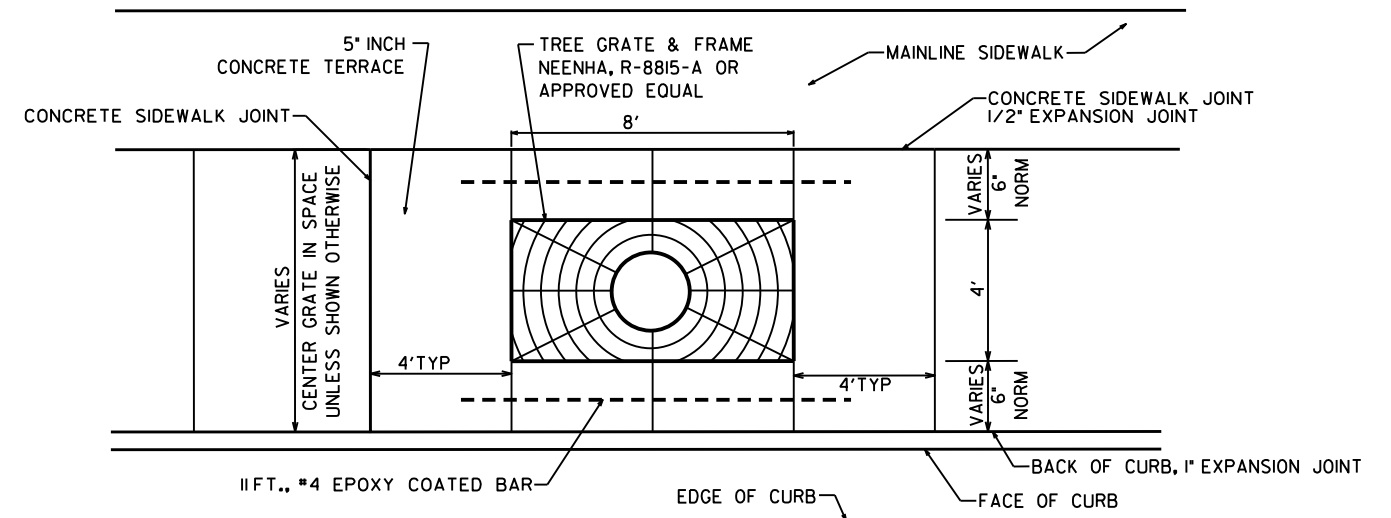


TREE GRATE CROSS SECTION DETAIL
NOT TO SCALE



TYPICAL SECTION

HIGHLAND AVE
NOT TO SCALE



TREE GRATE DETAIL
NOT TO SCALE

NOTES:

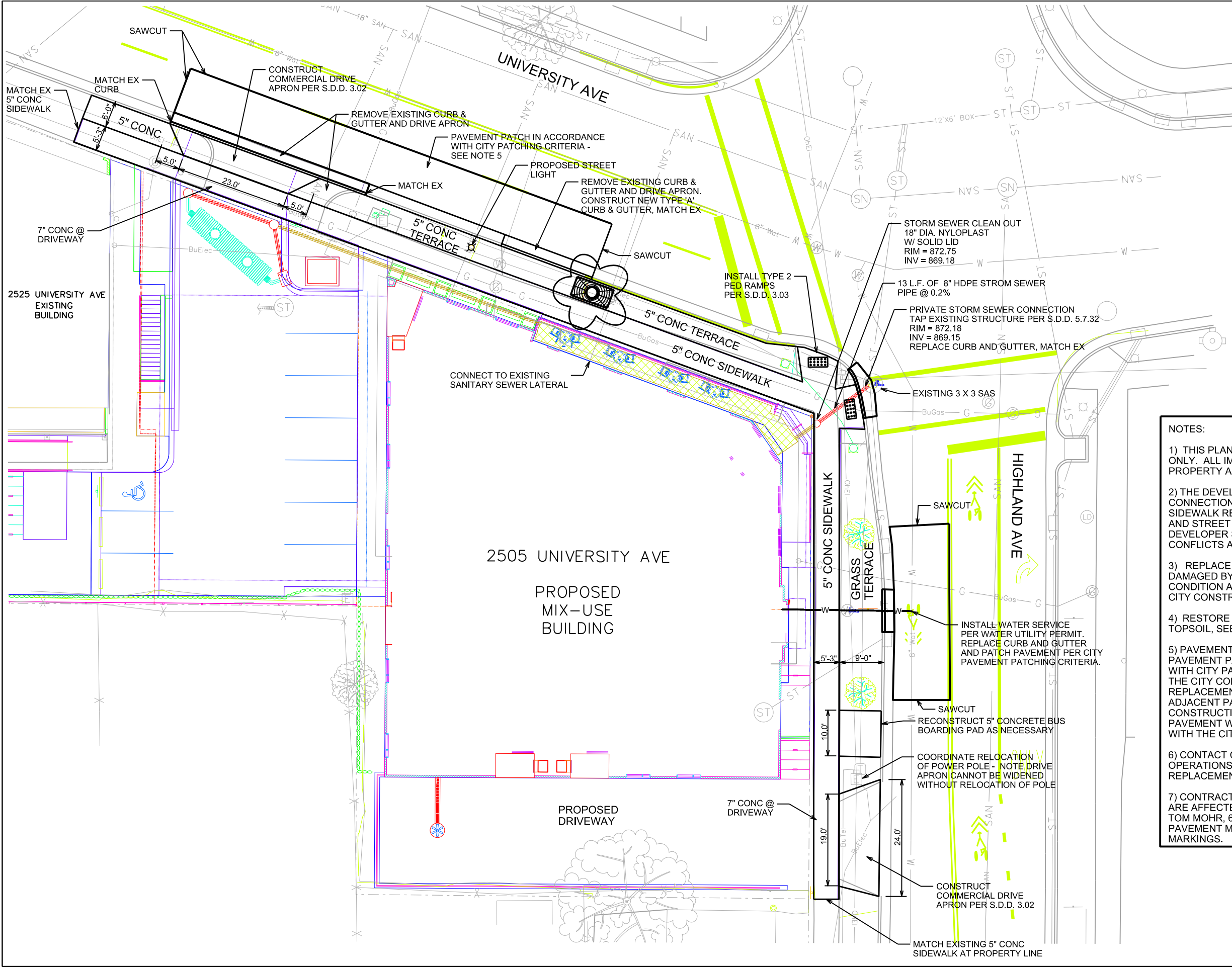
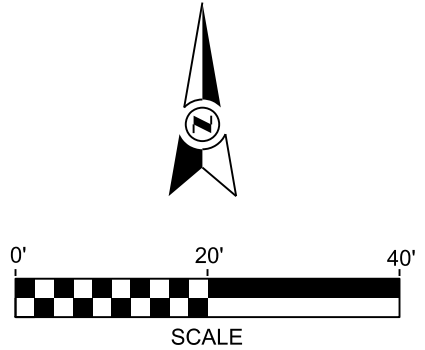
- * PAVEMENT PATCH LIMITS ARE APPROXIMATE. PATCH PAVEMENT PER CITY'S PATCHING CRITERIA. FINAL LIMITS ARE TO BE DETERMINED BY THE CONSTRUCTION ENGINEER.

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



- NOTES:
- 1) THIS PLAN IS FOR WORK IN THE PUBLIC RIGHT-OF-WAY ONLY. ALL IMPROVEMENTS SHOWN ON PRIVATE PROPERTY ARE FOR REFERENCE ONLY.
 - 2) THE DEVELOPER IS RESPONSIBLE FOR ALL SITE UTILITY CONNECTIONS, PAVEMENT PATCHING, CURB & GUTTER & SIDEWALK REPLACEMENT, STREET LIGHTING IMPROVEMENTS AND STREET TREE PLANTINGS & TERRACE RESTORATION. DEVELOPER SHALL IDENTIFY AND RESOLVE ALL UTILITY CONFLICTS ASSOCIATED WITH DEVELOPMENT.
 - 3) REPLACE ALL SIDEWALK, CURB & GUTTER AND PAVEMENT DAMAGED BY CONSTRUCTION OR THAT IS IN AN UNSATISFACTORY CONDITION AS SHOWN ON THE PLAN AND AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER.
 - 4) RESTORE ALL DISTURBED SOD TERRACE AREAS WITH 4-INCHES TOPSOIL, SEED AND EROSION MATTING.
 - 5) PAVEMENT PATCHING SHOWN IN PLAN IS APPROXIMATE. ALL PAVEMENT PATCHING AND SAWCUTS SHALL BE IN ACCORDANCE WITH CITY PAVEMENT PATCHING CRITERIA AND AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. CURB AND GUTTER REPLACEMENT MAY BE ACCOMPLISHED WITHOUT REMOVAL OF ADJACENT PAVEMENT AT THE DISCRETION OF THE CONSTRUCTION ENGINEER. ANY DAMAGE TO THE ADJACENT PAVEMENT WILL REQUIRE PAVEMENT PATCHING IN ACCORDANCE WITH THE CITY PAVEMENT PATCHING CRITERIA.
 - 6) CONTACT CITY OF MADISON TRAFFIC ENGINEERING FIELD OPERATIONS, 1120 SAYLE STREET, 266-4767, FOR REMOVAL AND REPLACEMENT OF TRAFFIC AND PARKING SIGNS.
 - 7) CONTRACTOR SHALL REPLACE ANY PAVEMENT MARKINGS THAT ARE AFFECTED BY CONSTRUCTION ACTIVITIES. CONTACT TOM MOHR, 608-267-8725, TMOHR@CITYOFMADISON.COM, FOR PAVEMENT MARKING PLAN PRIOR TO REPLACING PAVEMENT MARKINGS.

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

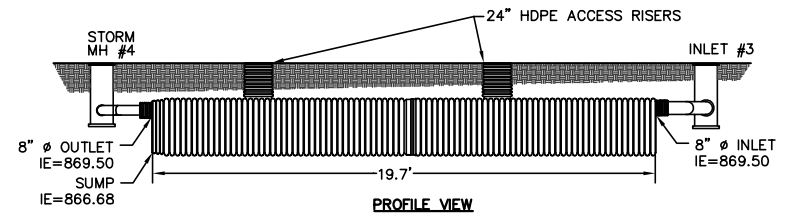
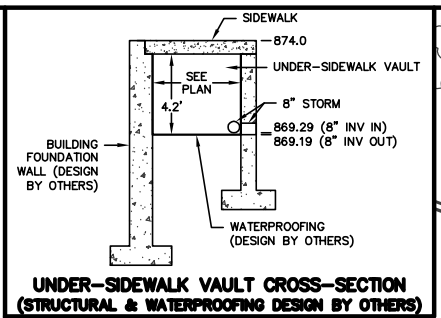
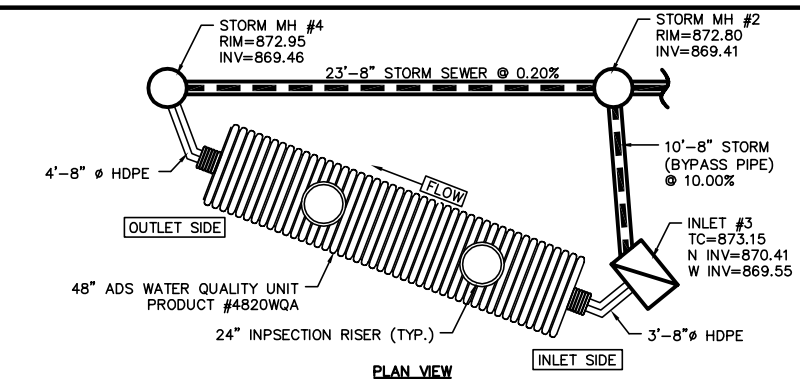
OLD UNIVERSITY REDEVELOPMENT
GRADING AND EROSION CONTROL PLAN **C1.3**
 DATED: AUGUST 4, 2015
QUAM ENGINEERING, LLC
 Residential and Commercial Site Design Consultants
 www.quamengineering.com
 4604 Siggelkow Road, Suite A; McFarland, Wisconsin 53558
 Phone (608) 838-7750; Fax (608) 838-7752

EROSION NOTES:
 THE STONE TRACKING PAD SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION. THE TRACKING PAD IS TO BE MAINTAINED BY THE CONTRACTOR IN A CONDITION, WHICH WILL PREVENT THE TRACK OF MUD OR DRY SEDIMENT ONTO THE ADJACENT PUBLIC STREETS. SEDIMENT REACHING THE PUBLIC ROAD SHALL BE REMOVED BY STREET CLEANING (NOT HYDRAULIC FLUSHING) BEFORE THE END OF EACH WORKDAY.
 EROSION CONTROL DEVICES SHALL BE INSTALLED PRIOR TO GRADING OPERATIONS AND SHALL BE PROPERLY MAINTAINED FOR MAXIMUM EFFECTIVENESS UNTIL VEGETATION IS ESTABLISHED. ALL EROSION CONTROL MEASURES AND STRUCTURES SERVING THE SITE MUST BE INSPECTED AT LEAST WEEKLY OR WITHIN 24 HOURS OF A 0.5 INCH RAIN EVENT. ALL MAINTENANCE WILL FOLLOW AN INSPECTION WITHIN 24 HOURS.
 CUT AND FILL SLOPES SHALL BE NO GREATER THAN 3:1.
 EROSION CONTROL IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ACCEPTANCE OF THIS PROJECT. EROSION CONTROL MEASURES AS SHOWN SHALL BE THE MINIMUM PRECAUTIONS THAT WILL BE ALLOWED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECOGNIZING AND CORRECTING ALL EROSION CONTROL PROBLEMS THAT ARE A RESULT OF CONSTRUCTION ACTIVITIES. ADDITIONAL EROSION CONTROL MEASURES, AS REQUESTED IN WRITING BY THE STATE OR LOCAL INSPECTORS, OR THE DEVELOPER'S ENGINEER, SHALL BE INSTALLED WITHIN 24 HOURS.
 CONTRACTOR SHALL PROVIDE A CONCRETE MANAGEMENT AND WASHOUT PLAN TO PREVENT CONCRETE WASTE FROM ENTERING THE CITY RIGHT-OF-WAY. PLAN SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL.
 CONTRACTOR SHALL PROVIDE A CONSTRUCTION DEWATERING PLAN IN CONFORMANCE WITH MWRN CONSERVATION PRACTICE STANDARD 1061. PLAN SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL.

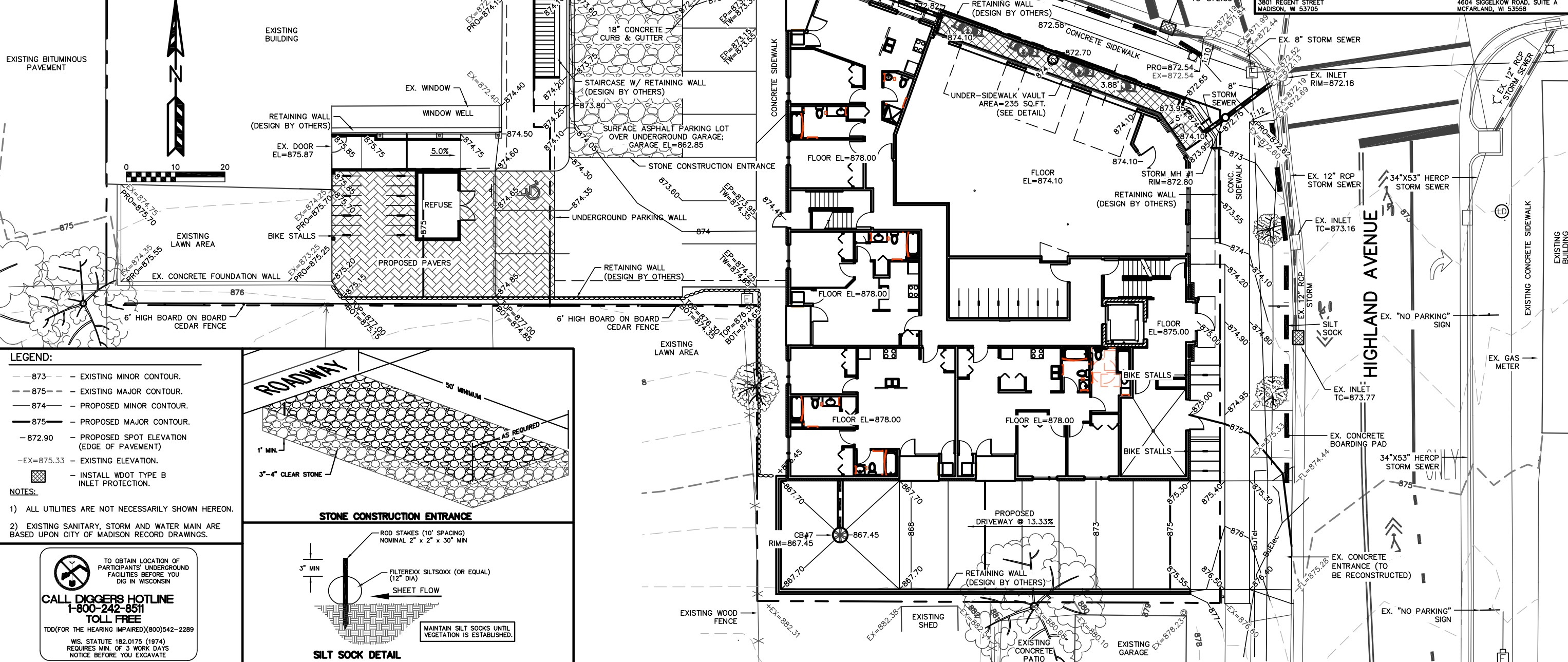
TIME SCHEDULE:
 JULY 2, 2015 - INSTALL EROSION CONTROL DEVICES.
 JULY 2, 2015 - JULY 2, 2016 - CONSTRUCT BUILDING, PARKING LOT, DRIVEWAY, AND SIDEWALK.

OWNER: LINDHOLM PROPERTIES, LLC
 3801 REGENT STREET
 MADISON, WI 53705

ENGINEER: QUAM ENGINEERING, LLC
 4604 SIGGELKOW ROAD, SUITE A
 MCFARLAND, WI 53558

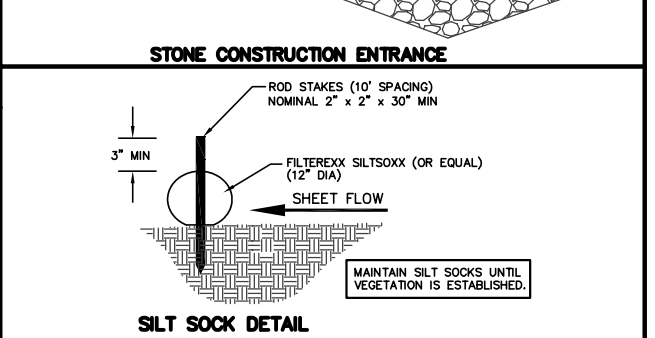
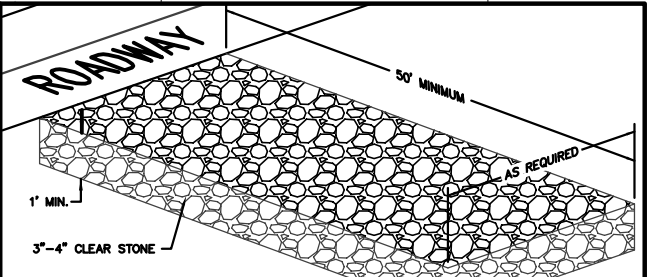


NOTES:
 1. SEE ADS STANDARD DETAILS STD-501 AND STD-504 FOR ADDITIONAL WATER QUALITY UNIT PRODUCT DETAILS.
 2. REFERENCE TECHNICAL NOTE 1.03 FOR ADDITIONAL DESIGN INFORMATION.



LEGEND:
 - - 873 - - EXISTING MINOR CONTOUR.
 - - 875 - - EXISTING MAJOR CONTOUR.
 - - 874 - - PROPOSED MINOR CONTOUR.
 - - 875 - - PROPOSED MAJOR CONTOUR.
 - 872.90 - PROPOSED SPOT ELEVATION (EDGE OF PAVEMENT)
 - EX=875.33 - EXISTING ELEVATION.
 [Symbol] - INSTALL WDOT TYPE B INLET PROTECTION.

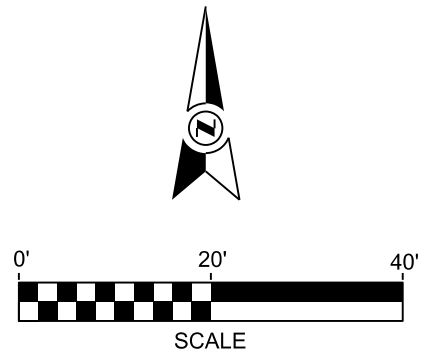
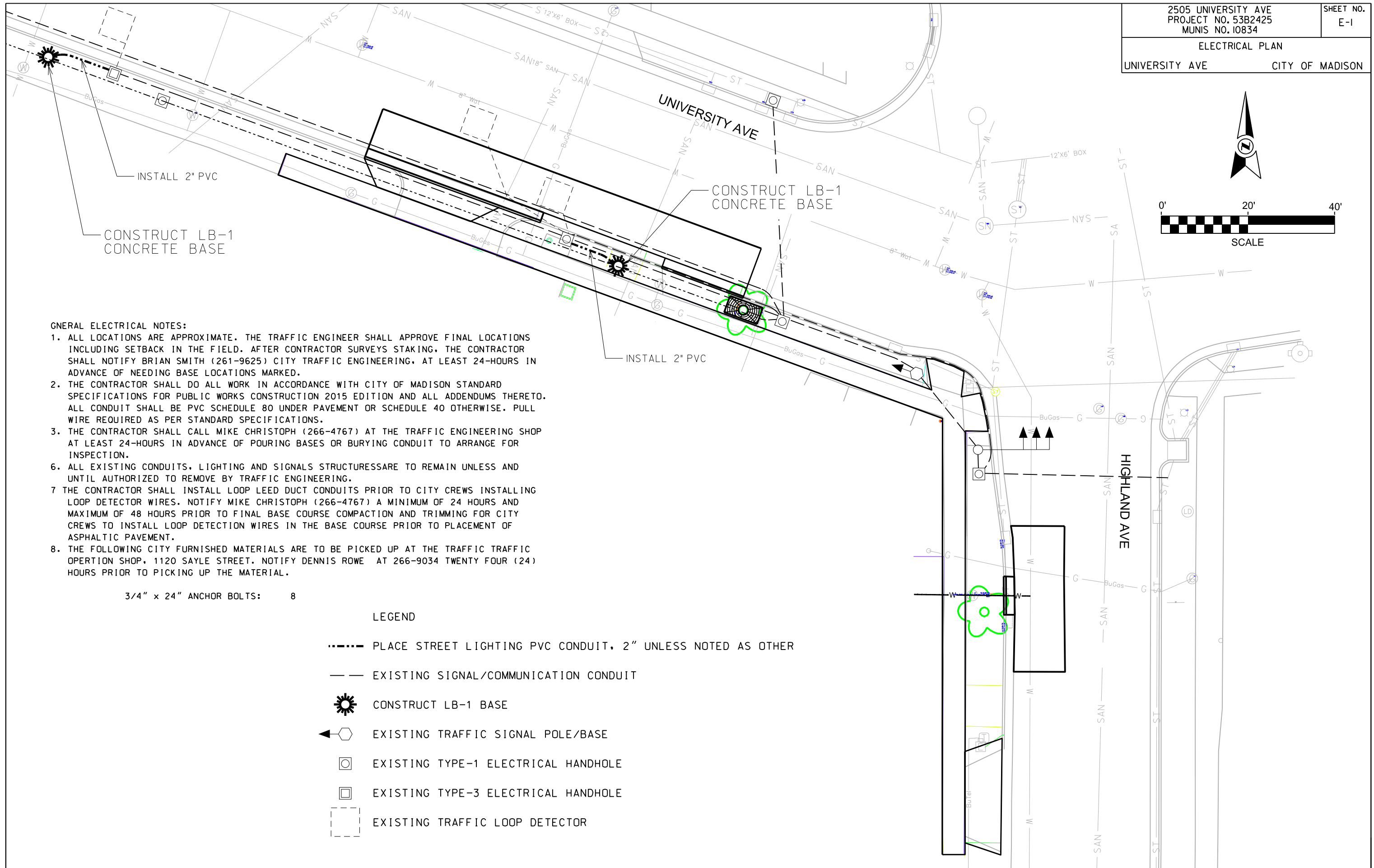
NOTES:
 1) ALL UTILITIES ARE NOT NECESSARILY SHOWN HEREON.
 2) EXISTING SANITARY, STORM AND WATER MAIN ARE BASED UPON CITY OF MADISON RECORD DRAWINGS.



CALL DIGGERS HOTLINE
 1-800-242-8511
 TOLL FREE
 TDD(FOR THE HEARING IMPAIRED)(800)542-2289

TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN

WS. STATUTE 182.0175 (1974)
 REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE



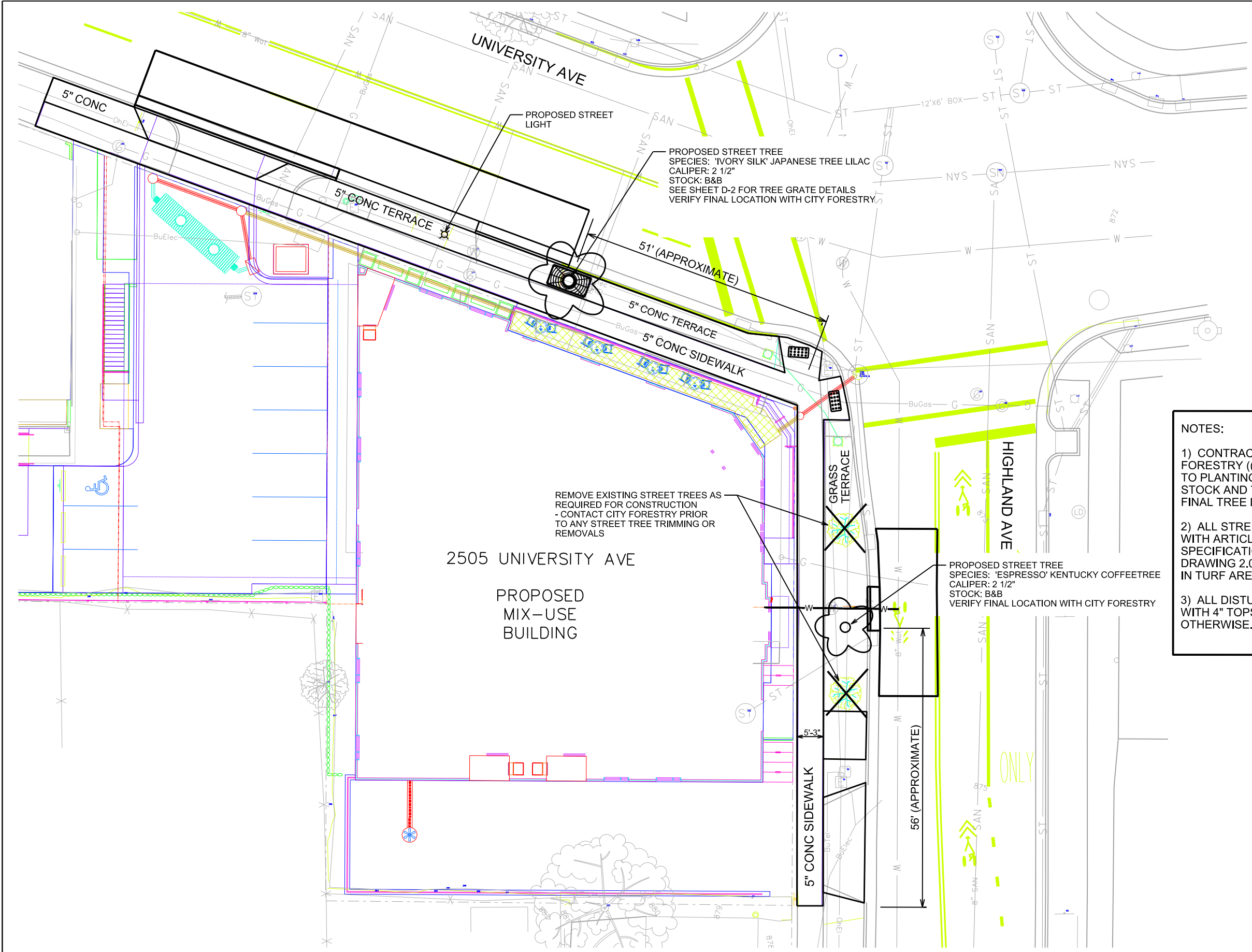
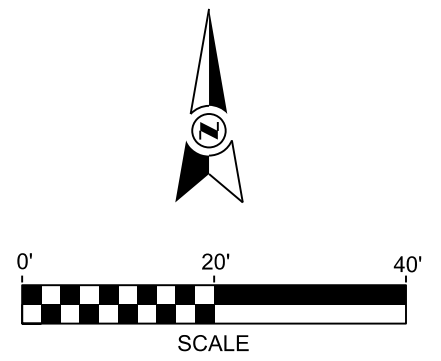
GENERAL ELECTRICAL NOTES:

1. ALL LOCATIONS ARE APPROXIMATE. THE TRAFFIC ENGINEER SHALL APPROVE FINAL LOCATIONS INCLUDING SETBACK IN THE FIELD. AFTER CONTRACTOR SURVEYS STAKING, THE CONTRACTOR SHALL NOTIFY BRIAN SMITH (261-9625) CITY TRAFFIC ENGINEERING, AT LEAST 24-HOURS IN ADVANCE OF NEEDING BASE LOCATIONS MARKED.
2. THE CONTRACTOR SHALL DO ALL WORK IN ACCORDANCE WITH CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION 2015 EDITION AND ALL ADDENDUMS THERETO. ALL CONDUIT SHALL BE PVC SCHEDULE 80 UNDER PAVEMENT OR SCHEDULE 40 OTHERWISE. PULL WIRE REQUIRED AS PER STANDARD SPECIFICATIONS.
3. THE CONTRACTOR SHALL CALL MIKE CHRISTOPH (266-4767) AT THE TRAFFIC ENGINEERING SHOP AT LEAST 24-HOURS IN ADVANCE OF POURING BASES OR BURYING CONDUIT TO ARRANGE FOR INSPECTION.
6. ALL EXISTING CONDUITS, LIGHTING AND SIGNALS STRUCTURES ARE TO REMAIN UNLESS AND UNTIL AUTHORIZED TO REMOVE BY TRAFFIC ENGINEERING.
7. THE CONTRACTOR SHALL INSTALL LOOP LEED DUCT CONDUITS PRIOR TO CITY CREWS INSTALLING LOOP DETECTOR WIRES. NOTIFY MIKE CHRISTOPH (266-4767) A MINIMUM OF 24 HOURS AND MAXIMUM OF 48 HOURS PRIOR TO FINAL BASE COURSE COMPACTION AND TRIMMING FOR CITY CREWS TO INSTALL LOOP DETECTION WIRES IN THE BASE COURSE PRIOR TO PLACEMENT OF ASPHALTIC PAVEMENT.
8. THE FOLLOWING CITY FURNISHED MATERIALS ARE TO BE PICKED UP AT THE TRAFFIC OPERATION SHOP, 1120 SAYLE STREET. NOTIFY DENNIS ROWE AT 266-9034 TWENTY FOUR (24) HOURS PRIOR TO PICKING UP THE MATERIAL.

3/4" x 24" ANCHOR BOLTS: 8

LEGEND

- PLACE STREET LIGHTING PVC CONDUIT, 2" UNLESS NOTED AS OTHER
- EXISTING SIGNAL/COMMUNICATION CONDUIT
- ☼ CONSTRUCT LB-1 BASE
- ◀◻ EXISTING TRAFFIC SIGNAL POLE/BASE
- ◻ EXISTING TYPE-1 ELECTRICAL HANDHOLE
- ◻ EXISTING TYPE-3 ELECTRICAL HANDHOLE
- - - - - EXISTING TRAFFIC LOOP DETECTOR



- NOTES:**
- 1) CONTRACTOR SHALL CONTACT DEAN KAHL OF CITY FORESTRY ((608) 266-4891) AT LEAST ONE (1) WEEK PRIOR TO PLANTING TO SCHEDULE INSPECTION OF NURSERY STOCK AND TO REVIEW PLANTING SPECIFICATIONS AND FINAL TREE LOCATIONS WITH THE LANDSCAPER.
 - 2) ALL STREET TREES SHALL BE PLANTED IN ACCORDANCE WITH ARTICLE 209 OF THE CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS AND STANDARD DETAIL DRAWING 2.01 - STANDARD PLANTING TECHNIQUE FOR TREES IN TURF AREAS.
 - 3) ALL DISTURBED TERRACE AREAS SHALL BE RESTORED WITH 4" TOPSOIL AND SEED OR SOD UNLESS NOTED OTHERWISE.

PLOT SCALE: _____
 PLOT NAME: _____
 REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION